

The Preserve at Keller Oaks

Keller, Texas

Highland Homes
Zoning Case #: ZONE-25XX-0XXX

Sage Group, Inc.
June 30, 2025

Request for Amendment to the City of Keller Zoning Plan

Today, we are excited to present to you “**The Preserve at Keller Oaks**,” a proposed Residential community of luxury homes in Keller, Texas.

The Preserve at Keller Oaks is intended to be a breathtaking community nestled atop Keller’s gently rolling landscape, where nature’s beauty and refined luxury converge in perfect harmony. Named to honor the majestic oak groves that define the land, this exclusive enclave will offer an elevated living experience amidst serene vistas and preserved natural surroundings.

Designed with a deep respect for the land’s inherent character, **The Preserve at Keller Oaks** blends seamlessly with its topography, safeguarding mature oak trees and creating a private, nature-filled sanctuary for residents. Here, spacious homes—crafted with exquisite attention to detail—exude timeless elegance and sophistication.

More than just a place to live, **The Preserve at Keller Oaks** will be a retreat for those who seek peace, privacy, and a profound connection to the natural world. It will stand as a distinguished addition to the city of Keller, elevating the community with a legacy of prestige, beauty, and thoughtful design for generations to come.

The Property:

The Preserve at Keller Oaks is located between the existing Harmonson Farms neighborhood (to the south) and the developing Greenway Park development (to the north), east of N. Main Street (US-377). The current, long-time owners of the property (the Perrigo family) are retaining a Commercial site of 400’ depth, along the frontage of N. Main- which is the same depth of the recently approved Greenway Park Commercial next to it. This Commercial site is not a part of this proposal. Also, in order to protect the development rights of our adjacent Commercial neighbor to the south, we are proposing for a small strip of land be retained under the Commercial zoning, as shown on the Concept Plan.

The proposal includes a neighborhood of 65 home sites and common open space areas on a total of 33.65 acres, a density of 1.98 homes per gross acre. Average lot size is 14,180 sf.

Existing Zoning/ Land Use Designation/Requested FLUP Amendment

The existing zoning designation attached to the property is partially C (Commercial), with the remainder designated SF-36. It is shown as MU (Mixed Use) and Medium-Density Residential on the Future Land Use Plan of the City of Keller. With this proposal we are requesting a change in the Future Land Use Plan designation to “HD-SF”, which is consistent with the adjacent Harmonson Farms neighborhood.

Proposed Zoning

The property within this submittal will be developed and used in accordance with the PD standards contained within the “Design Standards,” which are a part of this proposal.

Compatibility of Proposed Zoning with surrounding Zoning and Land Uses:

We feel the proposed zoning for this site will act as an effective transitional use between the Commercial Uses along US-377, the Medium-Density Residential lots to the north, and the existing smaller residential lots to the south.

Important Physical Features

The site generally drains from a high point “ridge” in the center of the property either toward the west (to US-377) or to the southeast (toward the drainage channel through Harmonson Farms).

Zoning Change Need

The zoning change is requested to best utilize this piece of property, as there is a need for this high-quality single-family housing in the Keller area. The City is becoming increasingly “built out” and development of tracts for lots of this size, is needed to meet the demand. We feel the proposed zoning for this site will be very compatible with the surrounding uses, and provide yet another great neighborhood to Keller.

Neighbor Outreach

During the entire zoning process, we have reached out to the residents and businesses in the area, to communicate our intentions and respond to their comments and questions. Such outreach has or will include knocking on doors, mailers to nearby homeowners, as well as a community meeting to present the project and answer questions in group form. In doing so, we have garnered much support, and are including a petition of support- detailing agreements- in our submission.

Adequacy of Public Facilities and Services

City water and sewer services are readily available at this site. The proposed subdivision will be served by an existing major water main within US-377 and the adjacent neighborhoods to the east and a waste water line located in US-377 and to our east.

The storm water drainage plan has been submitted with this proposal. All proposed infrastructure (roads, water, sewer, storm drainage) will be designed per City Code.

Home Owners Association

A Home Owners Association will be formed for the Residential portion, which will be responsible for the maintenance of all entry features, landscaping within Open Space Lots, and Open Space Lot Maintenance

Perimeter Screening, Fencing and Street Trees

As noted on the Concept Plan exhibit, there will be a masonry wall built along the western boundary with the adjacent Commercial site. Along the rear of Lots 27-35 (the only location where our proposed lots are adjacent to existing homes), we have proposed to work in concert with those homeowners to either retain or repair their existing fences, or install a new 8’ cedar fence, on metal poles in concrete foundations, with a cap.

All fences on Residential Lots, adjacent to open space lots, shall be of ornamental iron, and shall not extend forward of the front building setback line. All other fences on and between lots shall be 6’ cedar fences.

All City of Keller landscape ordinances, including street tree requirements, will be followed.

Sidewalks

A 5' concrete sidewalk shall be installed along all internal residential lot street frontages, and through the open space as shown on Zoning Concept Plan. The Home Builders shall install sidewalks in front of all residential lots with the construction of the homes; the developer shall install all other sidewalks during development.

Project Schedule / Phasing

Assuming zoning is granted, this project will proceed immediately to final design, platting and development, which is anticipated to be developed in one phase.

Keller – The Preserve at Keller Oaks

Design Standards

Zoning Case No. ZONE-2506-0007

The following design standards shall apply to **The Preserve at Keller Oaks** development:

All lots shall comply with the SF-12 District, Single-Family Detached Residential, with the following clarifications and revisions:

1. Minimum lots size shall be 12,000 sf.
2. Minimum lot width shall be 80'. On cul-de-sacs and/or elbows, the minimum lot width shall be 70'. Minimum lot width shall be established at the front yard setback.
3. Minimum lot depth shall be 125' deep.
4. Minimum Front Setbacks: 25 feet.
5. Minimum Side Yard Setbacks shall be 8'. Minimum separation between structures shall be 16'. Minimum side yard on a corner lot adjacent to a street shall be 15'.
6. Minimum Rear Yard Setback: 15', except along the southern boundary (lots 27-35) where is shall be 40'.
7. Minimum House Size: The homes on all lots shall have a minimum conditioned area of:
 - One story- 2,600 s.f.
 - Two story- 3,200 s.f.
8. Maximum Height: 2 ½ story/ 35' for the main building.
9. Maximum Lot Coverage: 45% for main building and 55% for main building and accessory buildings.
10. Exterior wall materials for residences
 - a. 100% masonry on the front elevation (brick, stone or cementitious siding).
 - b. 80% overall masonry per elevation.
11. Garage Requirements:
 - a. A minimum of a 2-car garage shall be provided in all homes;
 - b. A minimum of 75% of the homes shall have 2-car "J-Swing" garages. Consistent with Section 9.02.C.5 of the UDC, these homes shall also be allowed an additional single, front-facing garage with a maximum of seventy-two square feet (72 S.F.) of garage door exposure to the street, if located a minimum of forty-five feet (45') behind the property line or right-of-way, whichever is closer;
 - c. The remaining homes shall be allowed 2 car, "front-facing" garages, with a maximum of one-hundred forty-four square feet (144 S.F.) of garage door exposure to the street, with a minimum setback of 28', which is 3' further than the 25' front elevation setback.

12. Elevation Features (to be identified on building plans submitted for permits, to ensure compliance at the time of plan review)

- a. Each residence shall contain at least five (5) of the following 'Features':
 - i. Divided light windows on street facing elevations (front elevation and sides on corner lots)
 - ii. Enhanced masonry details (contrasting brick or stone patterns and/or changes in materials)
 - iii. Metal seam roof accents
 - iv. Shutter accents
 - v. Cast stone accents (cast stone features at entries or windows)
 - vi. At least 2 masonry materials (i.e. cementitious siding, brick and stone)
 - vii. Decorative coach lighting
 - viii. Pavers for sidewalk and/or driveway
 - ix. Low voltage landscape and path lighting
 - x. Cast stone address markers
 - xi. Enhanced carriage style garage doors
 - xii. Garage door hardware
 - xiii. Shed or gable dormers
 - xiv. Cedar accents such as brackets, corbels, columns, etc.
 - xv. Metal accents such as brackets, awnings, etc.
 - xvi. Applied metal address numbers
 - xvii. Box bay windows
 - xviii. Front porches

13. Roofing materials and pitch

- a. 5:12 minimum primary roof pitch. Roof materials shall be constructed of at least 40-year shingles. Three-tab shingles are prohibited.

14. Garage door materials and decorative design

- a. Raised panel, wood or decorative garage door(s) with opener.

15. Driveways shall be no wider than 20'.

16. Driveway surface materials (concrete, pavers, etc.)

- a. Finished concrete, broom finish with smooth border at joints, pavers, or stamped and/or stained concrete.

17. Landscaping and irrigation requirements for individual lots:

- a. All lots shall have full irrigation systems.
- b. All front yards shall be fully landscaped, with a minimum of either two (2) minimum 3" Canopy trees, or one (1) minimum 3" caliper Canopy and 2" caliper Understory tree (chosen from the below mentioned Exhibit A- Quality Tree lists for Large and Understory Trees), and 10 shrubs.
- c. Canopy Trees shall be a minimum of 3" caliper at installation, and shall be chosen from the list of tree species in the "Keller, Texas - Code of Ordinances; PART III - UNIFIED DEVELOPMENT CODE, Article Ten - TREE PRESERVATION, Section 10.02 – Tree Preservation Exhibits, Exhibit A- Quality Trees, Large Trees."

18. There shall be a minimum 6' wall built with the development adjacent to the future Commercial tract (behind lots 59-65), from the northern boundary, and wrapping along the south side of Lot 65, to the front setback line. The fence design along the southern boundary shall be determined in concert with the adjacent existing homeowners, to either retain/repair the existing fence, or build an up to a 8' pre-stained cedar, with metal poles, top cap, top side trim, new fence. All other lot lines adjacent to open space lots shall have 6' decorative metal fences, as shown on the Wall/Fence Diagram on the PD Concept Plan.
19. The development's open spaces shall be generally landscaped in accordance with the conceptual landscape plans provided and that more detailed landscape plans prepared by a Registered Landscape Architect shall be reviewed and approved prior to Final Plat approval. The open space landscaping and irrigation must be installed and operational prior to final inspection of the first home in the development.
20. Sidewalks:
 - a. Sidewalks shall be built along the street frontage of each lot by the home builder or developer, as shown on the Concept Plan exhibit. The sidewalk shall be five feet in width.
 - b. A 5' wide trail sidewalk shall be built by the developer within the open space corridor, as shown on the Concept Plan.
21. Cluster mailbox location and design
 - a. A cluster mailbox shall be provided, as per USPS standards.
22. Development entry sign location and design
 - a. The development shall have an Entry sign, located within the front open space/detention area, at the entry street intersection from SH-377.
23. All lateral and service lines for all utilities shall be placed and maintained underground.
24. An HOA shall be required and Common Area Open Space shall be maintained by the HOA.
25. Additional Requirements:
 - a.- No permanent use of temporary dwellings, such as recreational vehicles, travel trailers, or mobile homes, may be used for on-site dwelling purposes unless approved by a Specific Use Permit (SUP).
 - b.- Open Storage is prohibited (except for materials for the resident's personal consumption i.e. firewood, gardening materials, etc.
 - c.- Single-family homes with side entry garages have special setback requirements as established in Section 9.02 – Off-Street Parking and Loading Requirements.
 - d.- *Other Regulations* – As established in Section 9.01 – Building Design and Development Standards.

Street Lights: Street lights shall be selected from the Tri-County decorative street light options and shall meet city standards for location and minimum spacing.

Street Signs/ Enhanced Cluster Mailboxes:



Site Data Summary Chart

Single Family Residential Lots	65	
Open Spaces	7	
Residential Lots	64.42 %	21.17 ac.
Open Spaces	20.54 %	6.77 ac.
R.O.W.	15.04 %	4.93 ac.

Gross Acreage	100.00%	32.87 ac.
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Site Data

Gross Acreage	32.86 ac.
Gross Density	1.98/units per ac.

Lot Summary

Residential Lots	65
Minimum Lot Size	12,000 s.f.
Average Lot Size	14,180 s.f.

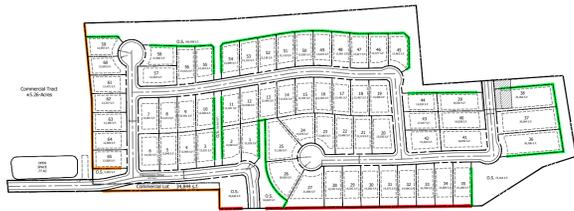
Commercial Site Data Summary Chart

Commercial Lots	1	
Commercial Lot	40.51 %	0.32 ac.
R.O.W.	59.49 %	0.49 ac.
Gross Acreage	100.00%	0.81 ac.

Total Area

Gross Acreage	33.68 ac.
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Fence / Wall Diagram



Fence Legend

- 6' Masonry Wall —
- Retain or repair existing fence; or replace with 8' Cedar Fence on metal poles with cap. —
- 6' Decorative Metal fence —



HOW TRUST IS BUILT™

Planner:

SAGE GROUP, INC.

- Master Planning
- Urban Design
- Architecture
- Landscape Architecture
- 1130 N. Carroll Ave., Ste. 200
- Southlake, Texas 76092
- Curtis Young
- TEL: 817-424-2626

01 AUG 25

1" = 100'



Detailed Site Plan



The Preserve at Keller Oaks

Keller, Tarrant County, Texas

60' Classic Elevation Feature Examples

Each Residence Shall Contain at Least Five (5)
out of Eighteen (18) Features, Per PD



HOW TRUST IS BUILT™

Elevation Feature Examples (min of 5 per elevation required)

1) Shutter Accents

2) Cast Stone Accents

3) Divided Light Windows

4) At Least 2 Masonry Materials

5) Decorative Coach Lighting

6) Front Porch

7) Cedar Accents/Columns

8) Shed or Gable Dormer

9) Enhanced Masonry Detail

10) Cast Stone Address Marker



Elevation Feature Examples (min of 5 per elevation required)

1) Shutter Accents

2) Cast Stone Accents

3) Divided Light Windows

4) Box Bay Windows

5) Enhanced Masonry Detail

6) Front Porch

7) Cedar Accents/Columns

8) Metal Seam Roof Accents

9) Applied Metal Address Numbers

10) Garage Door Hardware





33.68 ac.



McADAMS

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PHONE: 512-567-4504
EMAIL: chip.boyd@highlandhomes.com

Existing Conditions Map
33.205 ACRES

in the
RICHARD F. ALLEN SURVEY, ABSTRACT NO. 29
CITY OF KELLER
TARRANT COUNTY, TEXAS



GRAPHIC SCALE
0 50 100 200
1 inch = 100 ft.

REVISIONS

NO.	DATE	DESCRIPTION
06-17-2025		EXISTING CONDITIONS

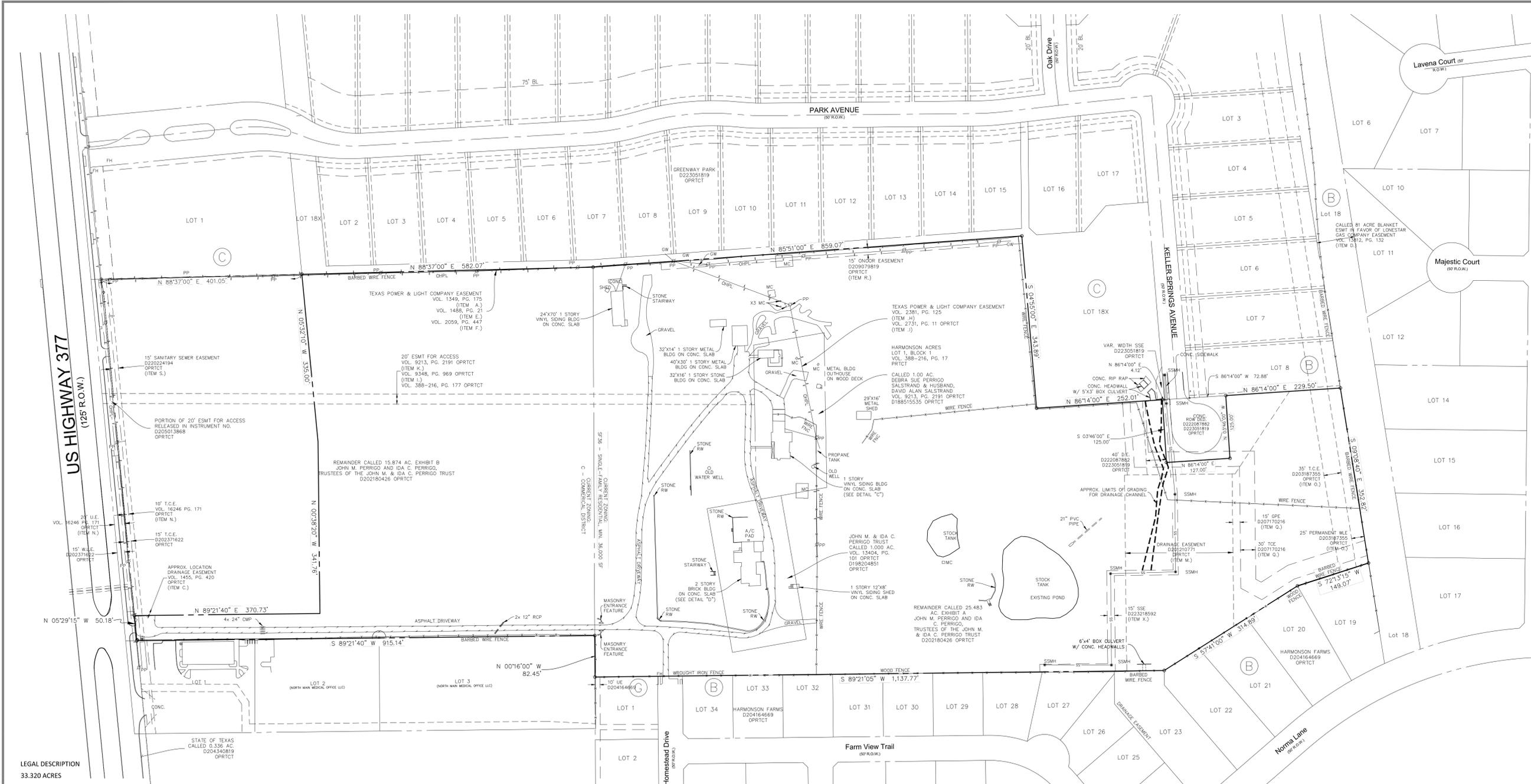
PLAN INFORMATION

PROJECT NO. PERRIGO TRACT
FILENAME SPEC 25109 PERRIGO TRACT

CHECKED BY PF
DRAWN BY
SCALE 100 SCALE
DATE JUNE 17, 2025

SHEET

EXISTING CONDITIONS
MAP
ECM



Being all that certain lot, tract or parcel of land situated in the Richard F. Allen Survey, Abstract Number 29, City of Keller, Tarrant County, Texas, and being part of that certain called 25.483 acre tract of land described in Exhibit A and part of that certain called 15.874 acre tract of land described in Exhibit B in deed in favor of John M. Perrigo and Ida C. Perrigo Trustee's of the John M. & Ida C. Perrigo Trust, recorded in Instrument Number D202180426 of the Official Public Records of Tarrant County, Texas, and being all of that certain called 1.00 acre tract of land described in deed in favor of Debra Sue Perrigo Salstrand and husband David Allen Salstrand, recorded in Volume 9213, Page 2191 (Instrument Number D188515535) of the Official Public Records of Tarrant County, Texas, and being all that certain called 1.00 acre tract of land described in deed in favor of John M. and Ida C. Perrigo Trust, recorded in Volume 13404, Page 101 (Instrument Number D198204851) of the Official Public Records of Tarrant County, Texas, and being all of Lot 1, Block 1 of Harmonson Acres, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Volume 388-216, Page 17 of the Plat Records of Tarrant County, Texas, and being all of Lot 1, Block A of Perrigo Place, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D223051819 of the Official Public Records of Tarrant County, Texas, and being part of Lots 1, 2 and 3, Block A of North Main Medical Office, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D221177168 of the Official Public Records of Tarrant County, Texas, and being more particularly described as follows:

COMMENCING at a disturbed 5/8" rebar originally recovered on January 7, 2021, (destroyed during construction of development to the north) and replaced with 1/2" capped rebar set (MCADAMS) on the north line of said 15.874 acre tract at the intersection of the east line of U. S. Highway 377, being the southwest corner of Lot 1, Block A of Greenway Park, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D223051819 of the Official Public Records of Tarrant County, Texas, and being the southeast corner of that certain called 0.281 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204348631 of the Official Public Records of Tarrant County, Texas, and being the northeast corner of that certain called 0.370 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D205013869 of the Official Public Records of Tarrant County, Texas;

THENCE N 88°37'00" E, 401.05 feet with the north line of said 15.874 acre tract and the south line of said Lot 1, Block A of Greenway Park to a 1/2" capped rebar found (MCADAMS) at the southeast corner thereof, being the most westerly southwest corner of Lot 18X, Block A of said Greenway Park, and being the POINT OF BEGINNING;

THENCE N 88°37'00" E, 582.07 feet continuing with the north line of said 15.874 acre tract and the south line of said Lot 18X to a 3/4" pipe found at the northeast corner of said 15.874 acre tract, being the northwest corner of said 25.483 acre tract, and being the northwest corner of said Lot 1, Block A of Perrigo Place;

THENCE N 85°11'00" E, 859.07 feet continuing with the south line of said Lot 18X and the north line of said 25.483 acre tract and the north line of said Lot 1, Block A of Perrigo Place to a 5/8" capped rebar found (HANSEN 4786) at the most northerly northeast corner of said Lot 1, Block A of Perrigo Place, being a reentrant corner in the south line of said Lot 18X, being a salient corner in the north line of said 25.483 acre tract;

THENCE S 04°55'00" E, 343.89 feet continuing with the southerly west line of said Lot 18X and the northerly line of said 25.483 acre tract and said Lot 1, Block A of Perrigo Place to a 1/2" capped rebar set (MCADAMS) at the most southerly southwest corner of said Lot 18X, being the westerly reentrant corner in the north line of said 25.483 acre tract and the north line of said Lot 1, Block A of Perrigo Place;

THENCE N 86°14'00" E, 252.00 feet continuing with the south line of said Lot 18X and the northerly line of said 25.483 acre tract and the northerly line of said Lot 1, Block A of Perrigo Place to a 1/2" capped rebar set (MCADAMS) at the northwest corner of that certain called 0.364 acre right-of-way dedication shown on said plat of Perrigo Place, from which a 1/2" capped rebar set (MCADAMS) at the southeast corner of said Lot 18X bears N 86°14'00" E, 4.12 feet;

THENCE S 03°46'00" E, 125.00 feet with the west line of said 0.364 acre right-of-way dedication to a 1/2" capped rebar set (MCADAMS) at the southwest corner thereof;

THENCE N 86°14'00" E, 127.00 feet with the south line of said 0.364 acre right-of-way dedication to a 1/2" capped rebar set (MCADAMS) at the southeast corner thereof;

THENCE N 03°46'00" W, 125.00 feet with the east line of said 0.364 acre right-of-way dedication to a 1/2" capped rebar set (MCADAMS) at the northwest corner thereof, being on the north line of said 25.483 acre tract, the north line of said Lot 1, Block A of Perrigo Place and the south line of said Lot 8, Block B of said Greenway Park, from which a 1/2" capped rebar set (MCADAMS) at the southwest corner of said Lot 8 bears S 86°14'00" W, 72.88 feet;

THENCE N 86°14'00" E, 229.50 feet continuing with the northerly line of said 25.483 acre tract, the north line of said Lot 1, Block A of Perrigo Place and the south line of said Lot 8, Block B to a 1/2" capped rebar set (MCADAMS) at the southeast corner of said Lot 8, Block B and the most easterly northeast corner of said Lot 1, Block A of Perrigo Place, being on the easterly line of said 25.483 acre tract, and the west line of Lot 18, Block B of Harmonson Farms, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Cabinet A, Slide 9144 (Document Number D204164669) of the Plat Records of Tarrant County, Texas;

THENCE S 09°08'40" E, 352.82 feet continuing with the east line of said 25.483 acre tract and the west line of said Lot 18, Block B to a 1/2" capped rebar set (MCADAMS) at the most easterly southeast corner of said 25.483 acre tract, being the northeast corner of Lot 19, Block B of said Harmonson Farms;

THENCE S 72°13'15" W, 149.07 feet with the south line of said 25.483 acre tract and the north line of said Lot 19 to a 1/2" rebar found at the northwest corner of said Lot 20, Block B of said Harmonson Farms;

THENCE S 57°41'00" W, continuing with the south line of said 25.483 acre tract and the northwesterly line of said Lot 20, passing at 120.0 feet a 1/2" capped rebar found (PRECISE) at the most westerly corner of said Lot 20, being the most northerly corner of said Lot 21, Block B of said Harmonson Farms, continuing with the northwesterly line of said Lot 21, passing at 232.9 feet a 1/2" rebar found at the most westerly corner of said Lot 21, being the most northerly corner of Lot 22, Block B of said Harmonson Farms, continuing with the northwesterly line of said Lot 22 a total distance of 314.89 feet to a 1/2" rebar found at the most northerly northwest corner of said Lot 22, being the most northerly northeast corner of Lot 23, Block B of said Harmonson Farms;

THENCE S 89°21'05" W, 1,137.77 feet continuing with the south line of said 25.483 acre tract and the north line of said Harmonson Farms to a 1/2" rebar found at the southwest corner of said 25.483 acre tract and the most westerly northwest corner of said Harmonson Farms, being on the east line of the aforementioned Lot 3, Block A of North Main Medical Office, from which a 5/8" rebar found at the southeast corner of said Lot 3 bears S 00°16'00" E, 102.6 feet;

THENCE N 00°16'00" W, 82.45 feet with the west line of said 25.483 acre tract and the east line of said Lot 3, being the southeast corner of the aforementioned 15.874 acre tract;

THENCE S 89°21'40" W, with the south line of said 15.874 acre tract, passing at 430.6 feet and 0.7 feet north of line a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 3 and the northeast corner of the aforementioned Lot 2, Block A of North Main Medical Office, passing at 679.0 feet and 1.2 feet north of line a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 2 and the northeast corner of the aforementioned Lot 1, Block A of North Main Medical Office, continuing a total distance of 915.14 feet to a 1/2" capped rebar set (MCADAMS) on the east line of said U. S. Highway 377 at the southeast corner of the aforementioned 0.370 acre tract, and being the northeast corner of that certain called 0.095 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204399009 of the Official Public Records of Tarrant County, Texas, from which a Texas Department of Transportation monument found at a point of curvature on the east line of that certain called 0.336 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204340819 of the Official Public Records of Tarrant County, Texas, bears S 05°29'15" E, 460.55 feet, and from said 1/2" capped rebar set (MCADAMS), a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 1 bears N 13°49' E, 1.7 feet;

THENCE N 05°29'15" W, 50.18 feet with the east line of said U. S. Highway 377 and the east line of said 0.370 acre tract to a 1/2" capped rebar set (MCADAMS), from which a Texas Department of Transportation monument originally recovered on January 7, 2021, (destroyed during construction of sanitary sewer extension for development to the north) and replaced with a 1/2" capped rebar set (MCADAMS) bears N 05°29'15" W, 646.30 feet;

THENCE N 89°21'40" E, 370.73 feet to a 1/2" capped rebar set (MCADAMS);

THENCE N 00°38'20" W, 341.76 feet to a 1/2" capped rebar set (MCADAMS);

THENCE N 05°32'10" W, 335.00 feet to the POINT OF BEGINNING and containing approximately 33.320 acres of land.

Bearings based on Texas Coordinate System, North Central Zone (4202), NAD 83.

Field work performed during the month of January 2021.

PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION

Z:\2024\Projects\SPC\2025\SPC25109 Perrigo Tract\04-Production\Planning - Design\Drawings\CAD\SPC25109 - Perrigo Tract - Existing Conditions Exhibit.dwg, 6/17/2025 4:49:48 PM, Marina Brewer



McADAMS

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MR. CHIP BOYD
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FORT WORTH, TEXAS 76177
PHONE: 972. 789. 3500



**PERRIGO TRACT
PRELIMINARY SITE EVALUATION
N. MAIN STREET (HWY 377)
CITY OF KELLER, TX.**

PRELIMINARY PLANS
THIS DOCUMENT IS FOR
INTERIM REVIEW AND IS
NOT INTENDED FOR
CONSTRUCTION, BIDDING,
OR PERMIT PURPOSES.
McADAMS,
TBPE: 19762
DARREN ANDREWS,
P.E. #90067
DATE 06/20/2025

REVISIONS

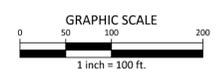
NO. DATE

PLAN INFORMATION

PROJECT NO. SPEC-25109
FILENAME SPEC25109-ZONING2
CHECKED BY JR
DRAWN BY KV
SCALE 1" = 100'
DATE 06. 20. 2025

SHEET

**PRELIMINARY
DRAINAGE STUDY
EX2**



DRAINAGE LEGEND

-  PROPOSED DRAINAGE AREA MAP ARROWS
-  PROPOSED CURB INLET
-  PROPOSED STORM SEWER LINE

Z:\Drive\Projects\SPEC\2025\SPEC25109 Perrigo Tract\04-Production\Engineering\Construction Drawings\Sheets\Preim Site Eval\SPEC25109-Zoning2.dwg, 6/17/2025 1:37:42 PM, Kevin Vo



McADAMS

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CLIENT

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MR. CHIP BOYD
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FORT WORTH, TEXAS 76177
PHONE: 972. 789. 3500



**PERRIGO TRACT
PRELIMINARY SITE EVALUATION
N. MAIN STREET (HWY 377)
CITY OF KELLER, TX,**

PRELIMINARY PLANS
THIS DOCUMENT IS FOR
INTERIM REVIEW AND IS
NOT INTENDED FOR
CONSTRUCTION, BIDDING,
OR PERMIT PURPOSES.
McADAMS,
TBPE: 19762
DARREN ANDREWS,
P.E. #90067
DATE 06/20/2025

REVISIONS

NO. DATE

PLAN INFORMATION

PROJECT NO. SPEC-25109
FILENAME SPEC25109-ZONING
CHECKED BY JR
DRAWN BY KV
SCALE 1" = 100'
DATE 06. 20. 2025

SHEET

**PRELIMINARY UTILITY
PLAN**

EX1



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION

Z:\Drive\Projects\SPEC\2025\SPEC25109 Perrigo Tract\04-Production\Engineering\Construction Drawings\Sheets\Prim Site Eval\SPEC25109-Zoning.dwg, 6/17/2025 12:57:16 PM, Kevin Vo

Trip Generation Data Form (Part 1)

Land Use/Building Type: ¹ SINGLE FAMILY DETACHED			ITE Land Use Code: 210		
Source: ITE TRIP GENERATION MANUAL 11TH EDITION			Source No. (ITE use only):		
Name of Development: Keller Springs			Day of the Week: Weekday		
City: Keller	State/Province: TX	Zip/Postal Code: 76248	Day: 17	Month: 06	Year: 2025
Country: United States			Metropolitan Area: Keller		

1. For fast-food land use, please specify if hamburger- or nonhamburger-based.

Location Within Area: <input type="checkbox"/> (1) CBD <input checked="" type="checkbox"/> (3) Suburban (Non-CBD) <input type="checkbox"/> (5) Rural <input type="checkbox"/> (2) Urban (Non-CBD) <input type="checkbox"/> (4) Suburban CBD <input type="checkbox"/> (6) Freeway Interchange Area (Rural) <input type="checkbox"/> (7) Not Given						Detailed Description of Development: ³					
Independent Variable: (include data for as many as possible) ² Actual Estimated Actual Estimated											
_____ (1) Employees (#)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (9) Parking Spaces (% occupied: _____)	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (2) Persons (#)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (10) Beds (% occupied: _____)	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (3) Total Units (#) (indicate unit: 65)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	_____ (11) Seats (#)	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (4) Occupied Units (#) (indicate unit: _____)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (12) Servicing Positions/Vehicle Fueling Positions	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (5) Gross Floor Area (gross sq. ft.) (% of development occupied _____)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (13) Shopping Center % Out-parcels/pads	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (6) Net Rentable Area (sq. ft.)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (14) A.M. Peak Hour Volume of Adjacent Street Traffic	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (7) Gross Leasable Area (sq. ft.) (% of development occupied _____)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (15) P.M. Peak Hour Volume of Adjacent Street Traffic	<input type="checkbox"/>	<input type="checkbox"/>						
_____ (8) Total Acres (% developed: _____)	<input type="checkbox"/>	<input type="checkbox"/>	_____ (16) Other _____	<input type="checkbox"/>	<input type="checkbox"/>						
			_____ (17) Other _____	<input type="checkbox"/>	<input type="checkbox"/>						

2. Definitions for several independent variables can be found in the Trip Generation, Second Edition, User's Guide Glossary.

3. Please provide all pertinent information to describe the subject project, including the presence of bicycle/pedestrian facilities. To report bicycle/pedestrian volumes, please refer to Part 4 of this data form.

Other Data: Vehicle Occupancy (#): A.M. _____ P.M. _____ 24-hour % _____ Percent by Transit: 0 A.M. % 0 P.M. % 0 24-hour % Percent by Carpool/Vanpool: 0 A.M. % 0 P.M. % 0 24-hour % Employees by Shift: First Shift: Start Time 0 End Time 0 Employees (#) 0 Second Shift: Start Time 0 End Time 0 Employees (#) 0 Third Shift: Start Time 0 End Time 0 Employees (#) 0 Parking Cost on Site: Hourly 0 Daily 0			Transportation Demand Management (TDM) Information: At the time of this study, was there a TDM program (that may have impacted the trip generation characteristics of this site) underway? <input checked="" type="checkbox"/> No <input type="checkbox"/> Yes (If yes, please check appropriate box/boxes, describe the nature of the TDM program(s) and provide a source for any studies that may help quantify this impact. Attach additional sheets if necessary) <input type="checkbox"/> (1) Transit Service <input type="checkbox"/> (5) Employer Support Measures <input type="checkbox"/> (9) Tolls and Congestion Pricing <input type="checkbox"/> (2) Carpool Programs <input type="checkbox"/> (6) Preferential HOV Treatments <input type="checkbox"/> (10) Variable Work Hours/Compressed Work Weeks <input type="checkbox"/> (3) Vanpool Programs <input type="checkbox"/> (7) Transit and Ridesharing Incentives <input type="checkbox"/> (11) Telecommuting <input type="checkbox"/> (4) Bicycle/Pedestrian Facilities and Site Improvements <input type="checkbox"/> (8) Parking Supply and Pricing Management <input type="checkbox"/> (12) Other _____		
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Please Complete Form on Other Side

 Institute of Transportation Engineers
Trip Generation Data Form (Part 2)

Summary of Driveway Volumes

(All = All Vehicles Counted, Including Trucks; Trucks = Heavy Duty Trucks and Buses)

	Average Weekday (M-F)						Saturday						Sunday					
	Enter		Exit		Total		Enter		Exit		Total		Enter		Exit		Total	
	All	Trucks	All	Trucks	All	Trucks	All	Trucks	All	Trucks	All	Trucks	All	Trucks	All	Trucks	All	Trucks
24-Hour Volume	306		307		613		308		308		616		276		275		551	
A.M. Peak Hour of Adjacent Street Traffic (7 – 9) Time (ex.: 7:15 - 8:15):																		
P.M. Peak Hour of Adjacent Street Traffic (4 – 6) Time:																		
A.M. Peak Hour Generator ² Time:	13		36		49													
P.M. Peak Hour Generator ² Time:	41		23		64													
Peak Hour Generator ³ Time (Weekend):							32		28		60		29		25		54	

¹ Highest hourly volume between 7 a.m. and 9 a.m. (4 p.m. and 6 p.m.). Please specify the peak hour.

² Highest hourly volume during the a.m. or p.m. period. Please specify the peak hour.

³ Highest hourly volume during the entire day. Please specify the peak hour.

Please refer to the *Trip Generation User's Guide* for full definition of terms.

Hourly Driveway Volumes- Average Weekday (M-F)

A.M. Period	Enter		Exit		Total		Mid-Day Period	Enter		Exit		Total		P.M. Period	Enter		Exit		Total	
	All	Trucks	All	Trucks	All	Trucks		All	Trucks	All	Trucks	All	Trucks		All	Trucks	All	Trucks	All	Trucks
6:00-7:00							11:00-12:00							3:00-4:00						
6:15-7:15							11:15-12:15							3:15-4:15						
6:30-7:30							11:30-12:30							3:30-4:30						
6:45-7:45							11:45-12:45							3:45-4:45						
7:00-8:00							12:00-1:00							4:00-5:00						
7:15-8:15							12:15-1:15							4:15-5:15						
7:30-8:30							12:30-1:30							4:30-5:30						
7:45-8:45							12:45-1:45							4:45-5:45						
8:00-9:00							1:00-2:00							5:00-6:00						

Check if Part 3, 4 and/or additional information is attached.

Survey conducted by: Name: Daniel Simonds
 Organization: McAdams
 Address: 4400 State Highway 121, Suite 800
 City/State/Zip: Lewisville, TX 75056
 Telephone #: (972) 436-9712 Fax #: _____ E-mail: dandrews@mcadamsco.com

Please return to: Institute of Transportation Engineers
 Technical Projects Division
 1627 I ST NW, STE 550
 Washington, DC, 20006, USA
 Telephone: +1 202-289-0222

ite Institute of Transportation Engineers
Trip Generation Data Form (Part 3)

Name/Organization: _____ City/State: _____

Telephone Number: _____

Detailed Driveway Volumes: Attach this sheet to Parts 1 and 2 if you are providing additional information.

Day of the week: _____ (All = All Vehicles Counted, Including Trucks; Trucks = Heavy Duty Trucks and Buses)

A.M. Period	Enter		Exit		Total		P.M. Period	Enter		Exit		Total	
	All	Trucks	All	Trucks	All	Trucks		All	Trucks	All	Trucks	All	Trucks
12:00-12:15							12:00-12:15						
12:15-12:30							12:15-12:30						
12:30-12:45							12:30-12:45						
12:45-1:00							12:45-1:00						
1:00-1:15							1:00-1:15						
1:15-1:30							1:15-1:30						
1:30-1:45							1:30-1:45						
1:45-2:00							1:45-2:00						
2:00-2:15							2:00-2:15						
2:15-2:30							2:15-2:30						
2:30-2:45							2:30-2:45						
2:45-3:00							2:45-3:00						
3:00-3:15							3:00-3:15						
3:15-3:30							3:15-3:30						
3:30-3:45							3:30-3:45						
3:45-4:00							3:45-4:00						
4:00-4:15							4:00-4:15						
4:15-4:30							4:15-4:30						
4:30-4:45							4:30-4:45						
4:45-5:00							4:45-5:00						
5:00-5:15							5:00-5:15						
5:15-5:30							5:15-5:30						
5:30-5:45							5:30-5:45						
5:45-6:00							5:45-6:00						
6:00-6:15							6:00-6:15						
6:15-6:30							6:15-6:30						
6:30-6:45							6:30-6:45						
6:45-7:00							6:45-7:00						
7:00-7:15							7:00-7:15						
7:15-7:30							7:15-7:30						
7:30-7:45							7:30-7:45						
7:45-8:00							7:45-8:00						
8:00-8:15							8:00-8:15						
8:15-8:30							8:15-8:30						
8:30-8:45							8:30-8:45						
8:45-9:00							8:45-9:00						
9:00-9:15							9:00-9:15						
9:15-9:30							9:15-9:30						
9:30-9:45							9:30-9:45						
9:45-10:00							9:45-10:00						
10:00-10:15							10:00-10:15						
10:15-10:30							10:15-10:30						
10:30-10:45							10:30-10:45						
10:45-11:00							10:45-11:00						
11:00-11:15							11:00-11:15						
11:15-11:30							11:15-11:30						
11:30-11:45							11:30-11:45						
11:45-12:00							11:45-12:00						

ite Institute of Transportation Engineers
Trip Generation Data Form (Part 4)

Summary of Bicycle Volumes

	Average Weekday (M-F)			Saturday			Sunday		
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
24-Hour Volume									
A.M. Peak Hour of Adjacent ¹ Street Traffic (7 – 9) Time (ex.: 7:15 - 8:15):									
P.M. Peak Hour of Adjacent ¹ Street Traffic (4 – 6) Time:									
A.M. Peak Hour Generator ² Time:									
P.M. Peak Hour Generator ² Time:									
Peak Hour Generator ³ Time (Weekend):									

¹ Highest hourly volume between 7 a.m. and 9 a.m. (4 p.m. and 6 p.m.) as defined in Trip Generation Data Form (Part 2). Please specify the peak hour.

² Highest hourly volume during the a.m. or p.m. period. Please specify the peak hour.

³ Highest hourly volume during the entire day. Please specify the peak hour. Please attach supplemental hourly volumes.

Please refer to the *Trip Generation User's Guide* for full definition of terms.

Summary of Pedestrian Volumes

	Average Weekday (M-F)			Saturday			Sunday		
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
24-Hour Volume									
A.M. Peak Hour of Adjacent ¹ Street Traffic (7 – 9) Time (ex.: 7:15 - 8:15):									
P.M. Peak Hour of Adjacent ¹ Street Traffic (4 – 6) Time:									
A.M. Peak Hour Generator ² Time:									
P.M. Peak Hour Generator ² Time:									
Peak Hour Generator ³ Time (Weekend):									

Survey conducted by: Name: _____

Organization: _____

Address: _____

City/State/Zip: _____

Telephone #: _____ Fax #: _____ E-mail: _____

Please return to: Institute of Transportation Engineers
 Technical Projects Division
 1627 I ST NW, STE 550
 Washington, DC, 20006, USA
 Telephone: +1 202-289-0222

ITE on the Web: www.ite.org

LEGAL DESCRIPTION

33.684 ACRES

Being all that certain lot, tract or parcel of land situated in the Richard F. Allen Survey, Abstract Number 29, City of Keller, Tarrant County, Texas, and being part of that certain called 25.483 acre tract of land described in Exhibit A and part of that certain called 15.874 acre tract of land described in Exhibit B in deed in favor of John M. Perrigo and Ida C. Perrigo Trustee's of the John M & Ida C. Perrigo Trust, recorded in Instrument Number D202180426 of the Official Public Records of Tarrant County, Texas, and being all of that certain called 1.00 acre tract of land described in deed in favor of Debra Sue Perrigo Salstrand and husband David Allen Salstrand, recorded in Volume 9213, Page 2191 (Instrument Number D188515535) of the Official Public Records of Tarrant County, Texas, and being all that certain called 1.00 acre tract of land described in deed in favor of John M. and Ida C. Perrigo Trust, recorded in Volume 13404, Page 101 (Instrument Number D198204851) of the Official Public Records of Tarrant County, Texas, and being all of Lot 1, Block 1 of Harmonson Acres, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Volume 388-216, Page 17 of the Plat Records of Tarrant County, Texas, and being all of Lot 1, Block A of Perrigo Place, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D223051819 of the Official Public Records of Tarrant County, Texas, and being all of that certain called 0.364 acre right-of-way dedication shown on said plat of Perrigo Place, and being part of Lots 1, 2 and 3, Block A of North Main Medical Office, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D221177168 of the Official Public Records of Tarrant County, Texas, and being more particularly described as follows:

COMMENCING at a disturbed 5/8" rebar originally recovered on January 7, 2021, (destroyed during construction of development to the north) and replaced with 1/2" capped rebar set (MCADAMS) on the north line of said 15.874 acre tract at the intersection of the east line of U. S. Highway 377, being the southwest corner of Lot 1, Block A of Greenway Park, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Instrument Number D223051819 of the Official Public Records of Tarrant County, Texas, and being the southeast corner of that certain called 0.281 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204348631 of the Official Public Records of Tarrant County, Texas, and being the northeast corner of that certain called 0.370 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D205013869 of the Official Public Records of Tarrant County, Texas;

THENCE N 88°37'00" E, 401.05 feet with the north line of said 15.874 acre tract and the south line of said Lot 1, Block A of Greenway Park to a 1/2" capped rebar found (MCADAMS) at the

southeast corner thereof, being the most westerly southwest corner of Lot 18X, Block A of said Greenway Park, and being the POINT OF BEGINNING;

THENCE N 88°37'00" E, 582.07 feet continuing with the north line of said 15.874 acre tract and the south line of said Lot 18X to a 3/4" pipe found at the northeast corner of said 15.874 acre tract, being the northwest corner of said 25.483 acre tract, and being the northwest corner of said Lot 1, Block A of Perrigo Place;

THENCE N 85°51'00" E, 859.07 feet continuing with the south line of said Lot 18X and the north line of said 25.483 acre tract and the north line of said Lot 1, Block A of Perrigo Place to a 5/8" capped rebar found (HANSEN 4786) at the most northerly northeast corner of said Lot 1, Block A of Perrigo Place, being a reentrant corner in the south line of said Lot 18X, being a salient corner in the north line of said 25.483 acre tract;

THENCE S 04°55'00" E, 343.89 feet continuing with the southerly west line of said Lot 18X and the northerly line of said 25.483 acre tract and said Lot 1, Block A of Perrigo Place to a 1/2" capped rebar set (MCADAMS) at the most southerly southwest corner of said Lot 18X, being the westerly reentrant corner in the north line of said 25.483 acre tract and the north line of said Lot 1, Block A of Perrigo Place;

THENCE N 86°14'00" E, continuing with the south line of said Lot 18X and the northerly line of said 25.483 acre tract and the northerly line of said Lot 1, Block A of Perrigo Place, passing the northwest corner of said 0.364 acre tract, passing the southeast corner of said Lot 18X, same being the southwest corner of a certain called 50' right-of-way dedication (Keller Springs Avenue) according to said plat of Greenway Park, passing the southeast corner thereof, same being the southwest corner of Lot 8, Block B of said Greenway Park, passing the northeast corner of said 0.364 acre tract, continuing a total distance of 608.50 feet to a 1/2" capped rebar set (MCADAMS) at the southeast corner of said Lot 8, Block B and the most easterly northeast corner of said Lot 1, Block A of Perrigo Place, being on the easterly line of said 25.483 acre tract, and the west line of Lot 18, Block B of Harmonson Farms, an addition to the City of Keller, Tarrant County, Texas, according to the plat thereof recorded in Cabinet A, Slide 9144 (Document Number D204164669) of the Plat Records of Tarrant County, Texas;

THENCE S 09°08'40" E, 352.82 feet continuing with the east line of said 25.483 acre tract and the west line of said Lot 18, Block B to a 1/2" capped rebar set (MCADAMS) at the most easterly southeast corner of said 25.483 acre tract, being the northeast corner of Lot 19, Block B of said Harmonson Farms;

THENCE S 72°13'15" W, 149.07 feet with the south line of said 25.483 acre tract and the north line of said Lot 19 to a 1/2" rebar found at the northwest corner of said Lot 19, being the most northerly corner of Lot 20, Block B of said Harmonson Farms;

THENCE S 57°41'00" W, continuing with the south line of said 25.483 acre tract and the northwesterly line of said Lot 20, passing at 120.0 feet a 1/2" capped rebar found (PRECISE) at the most westerly corner of said Lot 20, being the most northerly corner of Lot 21, Block B of said Harmonson Farms, continuing with the northwesterly line of said Lot 21, passing at 232.9 feet a 1/2" rebar found at the most westerly corner of said Lot 21, being the most northerly corner of Lot 22, Block B of said Harmonson Farms, continuing with the northwesterly line of said Lot 22 a total distance of 314.89 feet to a 1/2" rebar found at the most northerly northwest corner of said Lot 22, being the most northerly northeast corner of Lot 23, Block B of said Harmonson Farms;

THENCE S 89°21'05" W, 1,137.77 feet continuing with the south line of said 25.483 acre tract and the north line of said Harmonson Farms to a 1/2" rebar found at the southwest corner of said 25.483 acre tract and the most westerly northwest corner of said Harmonson Farms, being on the east line of the aforementioned Lot 3, Block A of North Main Medical Office, from which a 5/8" rebar found at the southeast corner of said Lot 3 bears S 00°16'00" E, 102.6 feet;

THENCE N 00°16'00" W, 82.45 feet with the west line of said 25.483 acre tract and the east line of said Lot 3, to a 1/2" rebar found at the northeast corner of said Lot 3, being the southeast corner of the aforementioned 15.874 acre tract;

THENCE S 89°21'40" W, with the south line of said 15.874 acre tract, passing at 430.6 feet and 0.7 feet north of line a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 3 and the northeast corner of the aforementioned Lot 2, Block A of North Main Medical Office, passing at 679.0 feet and 1.2 feet north of line a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 2 and the northeast corner of the aforementioned Lot 1, Block A of North Main Medical Office, continuing a total distance of 915.14 feet to a 1/2" capped rebar set (MCADAMS) on the east line of said U. S. Highway 377 at the southeast corner of the aforementioned 0.370 acre tract, and being the northeast corner of that certain called 0.095 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204399009 of the Official Public Records of Tarrant County, Texas, from which a Texas Department of Transportation monument found at a point of curvature on the east line of that certain called 0.336 acre tract of land described in deed in favor of the State of Texas, recorded in Instrument Number D204340819 of the Official Public Records of Tarrant County, Texas, bears S 05°29'15" E, 460.55 feet, and from said 1/2" capped rebar set (MCADAMS), a 1/2" capped rebar found (WINDROSE) at the northwest corner of said Lot 1 bears N 13°49' E, 1.7 feet;

THENCE N 05°29'15" W, 50.18 feet with the east line of said U. S. Highway 377 and the east line of said 0.370 acre tract to a 1/2" capped rebar set (MCADAMS), from which a Texas Department of Transportation monument originally recovered on January 7, 2021, (destroyed during

construction of sanitary sewer extension for development to the north) and replaced with a 1/2" capped rebar set (MCADAMS) bears N 05°29'15" W, 646.30 feet;

THENCE N 89°21'40" E, 370.73 feet to a 1/2" capped rebar set (MCADAMS);

THENCE N 00°38'20" W, 341.76 feet to a 1/2" capped rebar set (MCADAMS);

THENCE N 05°32'10" W, 335.00 feet to the **POINT OF BEGINNING** and containing approximately 33.684 acres of land.